

COUNTRYSIDE VILLAGES SECONDARY PLAN SUB AREA 48-I Employment Lands (East) 48-I Community Design Guidelines (CDG) Document



Prepared by:
MBTW/ Watchorn

Prepared for:
Metrus Development Incorporated

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The text and images contained in this document reflect a conceptual representation of the intended vision and character of the proposed development within this block plan area. These guidelines incorporate current City standards, or approved alternative design standards (ADS's), as applicable at the time of approval of this document. Final designs for block plan elements such as streetscapes, gateway features, pathways, bridges, street lighting, street signs, road cross-sections, utility locations, fencing and associated construction standards etc., may change over time. Changes may be permitted, subject to City approval, due to amendments to City standards, changes in technology, safety and/or construction codes, changes necessitated by the availability of identified materials or modifications to maintenance practices, etc.

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CONTENTS

| | | |
|------------|--|-----------|
| 1.0 | Introduction | 01 |
| 1.1 | Purpose | |
| 1.2 | Civic Planning and Design Initiatives | |
| 1.3 | Applicability of this Document to the City's DDG;s | |
| 1.4 | Site and Context | |
| 1.5 | The Vision | |
| 1.6 | Employment Centre Concept Sketch | |
| 2.0 | Community's Structuring Elements and Special Character Areas | 09 |
| 2.1 | Community Structuring Elements | |
| 2.1.1 | The Guiding Principles | |
| 2.1.2 | Street Network | |
| 2.1.3 | Views and Vistas | |
| 2.1.4 | Open Space System and Amenities | |
| 2.2 | Special Character Areas | 17 |
| 2.2.1 | Street I Village Commercial Centre (Terminus at Business Park Intersection) | |
| 2.2.2 | Hwy. 410 Edge | |
| 2.2.2.1 | Auto Mall Special Policy Area | |
| 2.2.3 | Arterial Road Edges | |
| 2.2.4 | Wetland Area | |
| 2.2.5 | Major and Minor Gateways | |
| 2.2.6 | Significant Intersections | |
| 3.0 | Site Planning and Built Form Principles | 33 |
| 3.1 | Site Planning and Design | |
| 3.2 | Building Form and Massing | |
| 3.3 | Building Elevations | |
| 3.4 | Building Entrances | |
| 3.5 | Pedestrian Circulation | |
| 3.6 | Vehicular Access, Parking and Servicing | |
| 3.7 | Loading, Servicing, Utilities and Outside Storage | |
| 4.0 | Landscape Guidelines | 37 |
| 4.1 | Goals, Objectives and Principles | |
| 4.2 | Landscape Zones | |
| 4.2.1 | Landscape Buffers and Screening | |
| 4.2.2 | Pedestrian Circulation | |
| 4.2.3 | Corners | |
| 4.2.4 | Gateways | |
| 4.2.5 | Stormwater Management Pond | |
| 4.2.6 | Channel Blocks and Open Space | |
| 4.2.7 | Parking | |
| 4.3 | Landscaping Elements | |
| 4.3.1 | Street Furniture | |
| 4.3.2 | Lighting | |
| 4.3.3 | Fencing Design Guidelines | |
| 4.3.4 | Signage | |
| 4.3.5 | Street Trees | |
| 5.0 | Implementation | 42 |
| 5.1 | Introduction | |
| 5.2 | Outstanding Work | |
| 5.3 | Conformity to the Community Design Guidelines (CDG) | |
| 5.4 | Cost Sharing | |
| 6.0 | Green Economic Principles | 44 |

1.0 INTRODUCTION

The planning for the Countryside Villages Community Secondary Plan area 48 began in 2004 resulting in the *Countryside Villages Design Brief* and *Countryside Villages Visions* documents (prepared by the MBTW Group). Building upon previous work, in 2009 STLA Inc. completed the *Countryside Villages Community Design and Open Space Study*. The *Study* derived a set of core community design principles to inform and guide the framework plan.

In 2009 the City of Brampton/BILD undertook a Development Process Review. Through this review process the City updated to include revised terms of reference for the Secondary Plan and Block Plan design documents. The Community Design Framework (CDF) outlines the vision and core principles at the Secondary Plan level and forms the basis for preparation of the Community Design Guidelines (CDG) at the Block Plan level. Since the 2009 *Community Design and Open Space Study* and all preceding documents set forth the vision and core design principles required under the CDF Terms of Reference it has been determined that the 2009 *Study* will form the basis for the *Countryside Villages Employment Lands (East) 48- Community Design Guidelines*.

Hereafter whenever a reference is made to the CDF it refers to the 2009 *Countryside Villages Community Design and Open Space Study*. While it was agreed that the formal Block Plan process will not apply to the lands west of Dixie Road, each of the two areas abutting Hwy 410 will be required to provide a Community Design Guidelines document for approval by the City.

1.1 Purpose

This document provides site specific guiding principles and criteria for Employment Lands East of Hwy 410 to satisfy requirements for the draft plan approval stage. Future site plan applications for this block will follow the principles and guidelines contained within this document. If it is determined, in the opinion of the City, that a site plan significantly deviates from these approved Community Design Guidelines, then a Design Brief will be provided by the applicant, for approval by the City.

The following common terms used in this document regarding design criteria are: 'shall/will', 'should', and 'encouraged/discouraged/may'. These terms are intended to have the following meaning with respect to compliance:

- 'Shall' / 'Will': Guidelines using the words 'shall' or 'will' are mandatory and must be provided.
- 'Should': Guidelines using the word 'should' are intended to be applied as stated. However, an alternative measure may be considered if it meets or exceeds the intent of the guideline.
- 'Encouraged' / 'Discouraged' / 'May': Guidelines using the words 'encouraged', 'discouraged', or 'may' are desirable but not mandatory.

I.2 Civic Planning and Design Initiatives

Notwithstanding the information contained within this Community Design Guidelines document, the following City of Brampton civic planning and design initiatives shall be implemented as applicable:

1. Six Pillars;
2. Clean and Green Strategy;
3. Accessibility Advisory Committee and Technical Standards;
4. Streetscapes Master Plan;
5. Flower City Strategy;
6. Gateway Beautification Program; and
7. Pathways Master Plan.

The implementation of the CDG is also dependent upon the completion of several supporting studies, including, but not limited to:

- Functional Servicing Report (FSR), prepared by Schaeffers Consulting Engineers;
- Environmental Impact Statement (EIS); and
- Cultural Heritage Impact Assessment, prepared by Scarlett Janusas Archaeological and Heritage Consulting and Education.

Three wetland areas were identified on site; figure 1.0 identifies the three wetlands area as Wetland Area A, a Provincially Significant Wetland (PSW), Wetland Area B, and Wetland Area C. Subject to EIS/FSR approval Wetland Area C will be removed and a Wetland Compensation Area is proposed immediately south of Wetland Area B. Since the Wetland Compensation Area and the location of the Stormwater Management Pond (SWM Pond) block are subject to approval of the FSR and EIS, the final design for the block plan will have regard for the recommendations of these studies and will not necessitate amendment to this CDG document.

I.3 Applicability of this Document to the City's DDG's

In keeping with the City of Brampton Development Design process, the plan meets all applicable parameters to guide development as outlined in the Development Design Guidelines section (DDGs) Manual (2003). The overarching design objectives and principles of Part III of the DDGs have been addressed in the planning, design and documentation prepared to date; whereas, this Community Design Guidelines document will address more specifically the objectives of Part V of the DDGs, as appropriate. **All guidelines found in this document are additional to the City's DDGs (2003) and are intended to enhance or heighten the overall design of the plan.** Areas subject to the Community Design Guidelines are:

- Community Structuring Elements, and
- Special Character Areas
 1. Street 1 - Village Commercial Centre
 2. Hwy 410 Edge
 3. Arterial Road Edges
 4. Wetland Area
 5. Major and Minor Gateways
 6. Significant Intersections

The principles of Policy 4, regarding accessibility, shall apply to all planning and design considerations within this Community Design Guidelines document:

Barrier free access to services and amenities is essential to achieving a truly vibrant City. The City has established the Accessibility Advisory Committee, and implemented the Accessibility Technical Standards to ensure that all residents of Brampton can live in a barrier free environment, including access to all City buildings. With the public sector taking the lead, the City shall promote barrier free access to private sector buildings and facilities as well as enforce the Ontario Building Code (OBC) related to the provision of barrier free access.

The City shall ensure that all new public buildings are accessible to persons with disabilities and ensure that existing public and private buildings are adapted to be accessible, in accordance with the OBC and the City of Brampton Accessibility Technical Standards.

CPTED principles, design for public safety, shall apply to all planning and design considerations within this Employment Lands area.

1.4 Site and Context

The Countryside Villages Employment Lands area is situated between Heart Lake Road to the west and Dixie Road to the east, Mayfield Road to the north and Countryside Drive to the south. The employment lands benefit from proximity to and high visibility from highway 410. The portion of the employment lands addressed by this document is located east of the Hwy 410 and is known as the Employment Lands (East) 48-1. The balance of employment lands represents a future development scenario and is known as the Employment Lands (West) 48-1.

The land subject to this Community Design Guidelines document and the submitted draft plan comprises an area of 78.5ha (194 acre). Located at the west end of the proposed Countryside Village community, the 410 Employment Centre will be an important local employment opportunity for the proposed community to the east and existing surrounding communities, as well as, regionally with convenient access from Hwy 410.

The owner of the 0.7 acre property at 11300 Dixie Road (CON 3 EHS PT LOT 16) (west of Dixie Road and north of proposed Street 4, see Figure 7.0) chose not to participate. The plan however addresses the non-participating property from an urban design perspective; ensuring that the surrounding landscape and built-form are sensitive to existing conditions.

Two properties at 11960 Dixie Road and 11098 Dixie Road have been identified as exhibiting potential cultural heritage significance according to the City of Brampton Heritage Guidelines. In 2010 Metrus Developments Inc. retained *Scarlett Janusas Archaeological and Heritage Consulting and Education* to complete a Cultural Heritage Impact Assessment for the properties in question. The archaeological Impact Assessment concluded the following:

- Both structures at 11960 Dixie Road and 11098 Dixie Road are not recommended for retention.
- The outbuilding structures at 11960 Dixie Road do exhibit certain elements that could be incorporated into future buildings, but the structures themselves are not considered significant.
- A plaque should be erected at the corner of Mayfield Road and Dixie Road commemorating the location of the former Crossroads of Mayfield.

The Crossroads of Mayfield was an inn located at the crossroads of Mayfield Road and Dixie Road. There is little historical information available regarding the inn; the original structures at the southwest corner are not visible above grade.

For further information refer to *Cultural Heritage Assessment 11960 and 11098 Dixie Road City of Brampton Regional Municipality of Peel report*, prepared by *Scarlett Janusas Archaeological and Heritage Consulting and Education* in June 2010 (Draft, subject to approval).

The physical characteristics of the employment lands east of Hwy 410 include:

- considerable land area without servicing limitations,
- existing surrounding infrastructure and services ready for extension into this area,
- access to and visibility from Hwy 410 (via Mayfield Road.), a regional transport network,
- direct access and visibility from Mayfield Road., a developing north GTA cross-connecting transportation corridor (a regional transport network),
- existing and proposed adjacent land uses which buffer and transition this area to residential development,
- proposed adjacent residential development that will benefit from this employment area,
- level to gently undulating topography principally used for agricultural purposes, and
- existing PSW area; the plan preserves wetlands and provides compensation for a removed wetland.

All of these attributes contribute to this employment zone as a desirable development area.

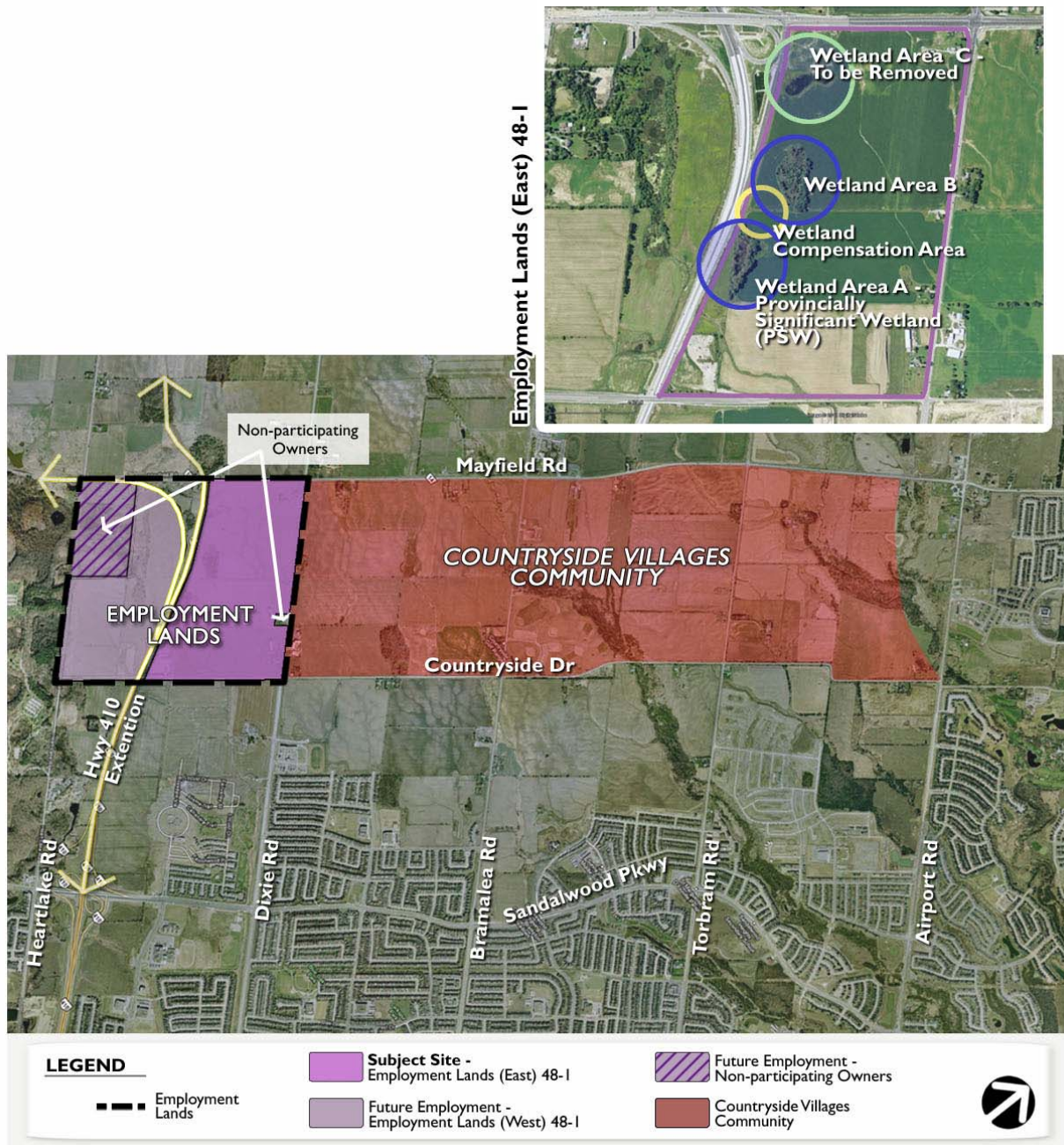


Figure 1.0 Site Context Source: Google, April 2009

1.5 The Vision

The 410 Employment Centre is proposed to be an upscale business park and a major employment location. As a vibrant and attractive mixed-use employment zone it will include office, light industrial and commercial uses. Anticipated development types should be located with regard to site opportunities and sensitivity to adjacent land uses, placing the more upscale developments closer to Hwy 410 and Mayfield Road, medium scale commercial/industrial uses in the centre of the zone and smaller scale office and commercial uses at its perimeter opposite residential areas.

The concept plan encourages New Urbanism principles and design strategies. The proposed plan integrates light industrial uses with office and commercial uses supporting a mixed use development located in relative proximity to homes. Office and commercial uses are sited adjacent to residences, while light industrial uses are situated closer to Hwy 410.

As a sustainable, well serviced, adaptable, and transit and pedestrian accessible employment zone, the 410 Employment Centre will also maintain and enhance the integrity of the existing natural heritage system by linking two wetlands with a naturalized compensation zone as well as linking to the Spring Creek Tributary (channel).

Location of the SWM block adjacent to the channel provides additional open space for habitat enhancement. And integration of possible private open space system and amenities with this open space and natural heritage system creates opportunity for an extensive resource for passive recreation. An existing natural heritage system is located west of Hwy 410 and continues along the eastern and southeastern boundaries of this site. The natural heritage system along with the SWM Pond provides an opportunity to develop an open space feature at the gateway to the Employment Lands from Countryside Drive, along the Hwy 410 edge, and at the terminus of Street I (Main Street), leading from the Countryside Villages residential community to the east.

Development within this Brampton north central employment zone should attract clean corporate enterprises, without the constraints of the GTAA airport overlay restrictions and rail service. As well, proposed developments located within the Countryside Villages Employment Lands have the opportunity to respond to societal concerns for energy footprint, Low Impact Development (LID) practices and LEED initiatives.

This vision builds upon Section 3.0 of the *Brampton Official Plan (2006)* for Countryside Villages Area and the Community Vision section from *Countryside Villages Community Design and Open Space Study (2009)*; to create a comprehensively planned strategic 410 Employment Centre encouraging a broad range of upscale office and industrial uses. This document demonstrates how the 410 Employment Centre will be comprehensively developed within the entire Countryside Villages community by providing specific guidelines and criteria to guide the future development of this block plan area.



Figure 1.1 Employment Centre Concept Sketch

2.0 COMMUNITY'S STRUCTURING ELEMENTS & SPECIAL CHARACTER AREAS

2.1 Community Structuring Elements

Structuring elements serve as the main planning components for delineating uses, establishing street hierarchy, and providing the development's framework by identifying key attributes such as gateways, views and vistas, connections and linkages, appropriate built form and landscape opportunities and responses. The community structure is illustrated in Figure 2.0. The key community structuring elements addressed by this section include:

- 2.1.1 The Guiding Principles
- 2.1.2 Street Network
- 2.1.3 Views/Vistas
- 2.1.4 Open Space System/Amenities

2.1.1 The Guiding Principles:

- Provide an efficient, safe and attractive space for pedestrians, transit users and private vehicles with a built form that has strong street orientation.
- Provide employment land blocks capable of adapting to individual development requirements and recognize the realistic needs of the business community for their efficient operation.
- Develop an open space system that connects to and enhances natural heritage resources and locates open space amenities equitably and with significant public exposure.
- Locate employment types and corresponding built form with sensitivity to surrounding and adjacent land uses and to respond to opportunities.
- Identify key community gateways and their role/introduction to the Employment Lands.
- Extend the role and character of Main Street into the centre of the employment zone to visually and functionally link the employment and residential communities of Countryside Villages.



LEGEND

Subject Lands

Future Employment Lands

Open Space

SWM Pond

Channel/Wetland

Major Gateways into the Employment Subdivision

Minor Gateways from Dixie Road

Business Park Intersection

Neighbourhood Centre

Class I Bike Lane Multi-Use (City of Brampton Proposed Trail Network)

Arterial Road Gateway

COUNTRYSIDE VILLAGES EMPLOYMENT LANDS EAST 48-1 (EAST OF HWY 410)
 CONCEPT PLAN & COMMUNITY STRUCTURING ELEMENTS
 June, 2011

mbtw watchorn

Figure 2.0 Concept Plan and Community Structuring Elements

2.1.2 Street Network

The block plan area will be serviced internally by a north-south collector road (Street 2) connecting Countryside Drive and Mayfield Road. There will be an important connection to this internal road by the extension of the Main Street (Street 1) also a collector road. These two roads, at 23m ROW will form the primary road network. This street network will provide safe pedestrian access and transit services. See Figure 2.0a for street network details.

Streets 3 and 4, although serving more local connection, are also proposed as 23m ROWs to facilitate the movement of truck traffic within the employment lands.

A secondary street network may be created with potential local roads and individual driveways. The primary and secondary road network will create a regular pattern of lot distribution while reinforcing street addressing. The street network will create efficient, feasible, attractive and pedestrian friendly streetscapes for a variety of uses and servicing functions, through the following principles:

- Safe and unobstructed vehicular movement integrated with active and safe pedestrian traffic.
- Streets and driveways that have safe and unobstructed access.
- Pathways should provide access to building entrances with unobstructed views of users.
- Hard landscaping treatments along Major Collector Roads should define street edge and highlight the public realm.



Figure 2.0a Street Network

2.1.3 Views & Vistas

Within the Employment Lands (as demonstrated in *Figure 3.0 Significant Views and Vistas*) significant views from Hwy 410, Mayfield Road, and Main Street (proposed development to the east) should inform planning and landscape design initiatives to focus views on desirable areas. The predominant visual character of this area will be views from the streets to adjacent buildings and open spaces, and views to the northern and southern portions of the easterly Hwy 410 edge.

Street alignments can be used to create vistas to important community elements by placing terminus features at the end of view corridors. The terminus of Main Street, from the Countryside Village residential community (Street 1) is an example of a vista created through street alignments.

One of the key attributes of the Employment Lands is the proximity to and visibility from Hwy 410. The following views were recorded:

- Angled views from the northbound lane are possible along the southern half of the Employment Lands, offering partial views to Block 2, and direct views of the Wetland Area and the proposed Open Space network (see [Figure 3.0, view 6](#)).
- Angled views from the northbound access ramp to Mayfield Road provide direct views to adjacent lands to the east (see [Figure 3.0, views 4 & 5](#)).
- Travelling the south bound lanes along the curve of the highway under the Mayfield Road overpass allows for partial views to the northerly Hwy 410 edge at Block 1 and partial views of Block 2 (beyond the Mayfield Road interchange access ramp where 2nd storey buildings may be visible) (see [Figure 3.0, view 3](#)).
- The northern portion of Hwy 410 edge is clearly visible from the eastbound Mayfield Road overpass. Raised sections of Mayfield Road east of Hwy 410 also provide significant views into the north half of the block (see [Figure 3.0, views 1 & 2](#)).
- The southern portion of Hwy 410 edge is clearly visible from the Countryside Drive overpass (see [Figure 3.0, view 7](#)).

Views of Blocks 1 and partial views of Blocks 2 and 6 support opportunities to situate the more prominent well designed building facades along Hwy 410 edge to enrich the City of Brampton business image. Encouraging two storey buildings with prominent massing, signage and higher order architectural detailing will further reinforce the upscale character of the area. As such, service areas, storage and parking are encouraged to be sufficiently screened or oriented in such a way as to be internalized along the building flankage.

Due to grades (at Countryside Drive and Hwy 410) and vegetation (through the Wetland Area) buildings in the south portion of Blocks 2 and all of Block 6 have limited visibility from Hwy 410; as such, special landscape and architectural treatments are encouraged to be oriented to the internal street (Street 2).

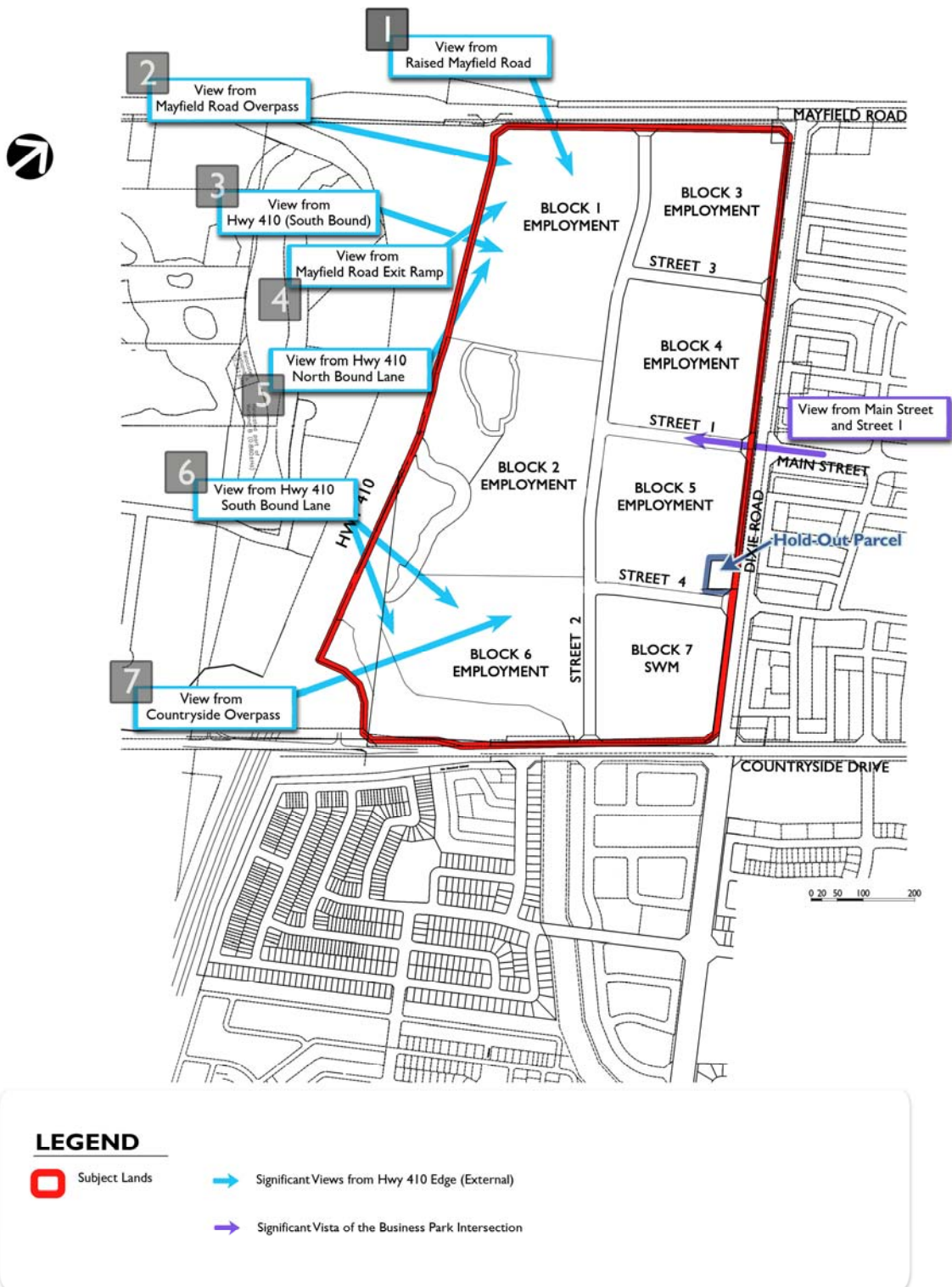


Figure 3.0 Significant Views & Vistas

2.1.4 Open Space System and Amenities

The Open Space and Amenities network consists of two preserved wetlands, a wetland compensation area (for a removed wetland), the Spring Creek Tributary (channel), a naturalized linkage between the wetland and the channel, and a proposed Stormwater Management Pond. The open space network provides opportunities for views and restoration of of Natural Heritage features.



Open Space

A focal lookout area at terminus of Street 1 (see Alternatives B and C) provides an opportunity to incorporate a private parkette or a plaza to capitalize on views of the natural heritage features (this is a Special Character Area described in greater detail in section 2.2.1). Proximity to Street 1 and 2 pedestrian routes adds visibility and access.

The proposed Stormwater Management facility area is located adjacent to the Countryside Drive and Dixie Road intersection and may provide, where ecologically feasible, opportunity for passive recreation. Maintenance access roads within the SWM block provide pedestrian access. The feature has abutting roads on four sides enabling significant public visibility and amenity. To facilitate passive recreational activity a small seating area overlook, themed to the open space network will be considered.

To maintain public safety, views of the pond and adjacent natural heritage areas from surrounding streets and public areas of private sites will be developed through the detail design process. As well, buildings opposite these areas should be designed with overlooking windows, especially from occupied second floors.

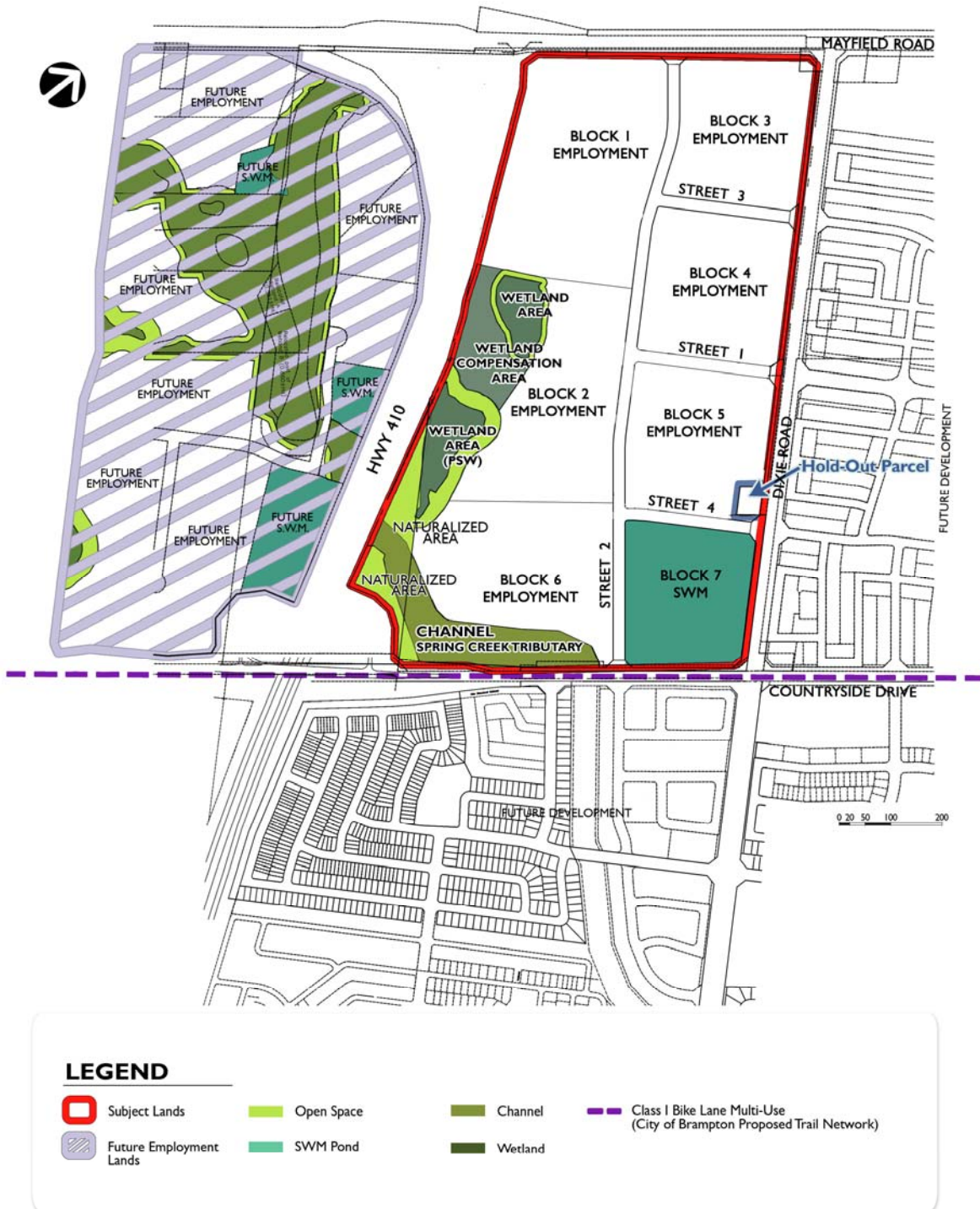


Figure 4.0 Open Space Network

2.2 Special Character Areas

The identity and character of the Employment Lands can be highlighted and reinforced with the creation of Special Character Areas. Six special character areas are proposed within the Countryside Villages Employment Lands East of Hwy 410.

Figure 5 demonstrates design principles and guidelines as discussed in this document, as well as identifying special character areas. Building footprint and parking area relationships are illustrative only and do not represent floor area to parking requirement ratios. It is expected that within the context of securing tenants for specific blocks, tenant needs in concert with City standards and these guidelines will inform the site plan design.

- 2.2.1 Street 1 - Village Commercial Centre (Terminus at Business Park Intersection)
- 2.2.2 Hwy 410 Edge
 - 2.2.2.1 Auto mall
- 2.2.3 Arterial Road Edges
- 2.2.4 Wetland Area
- 2.2.5 Major and Minor Gateways
- 2.2.6 Significant Intersections

Additional Special Character Areas may be defined in greater detail at the site plan design stage. These areas will deserve more detailed design consideration to highlight their distinctive characteristics.

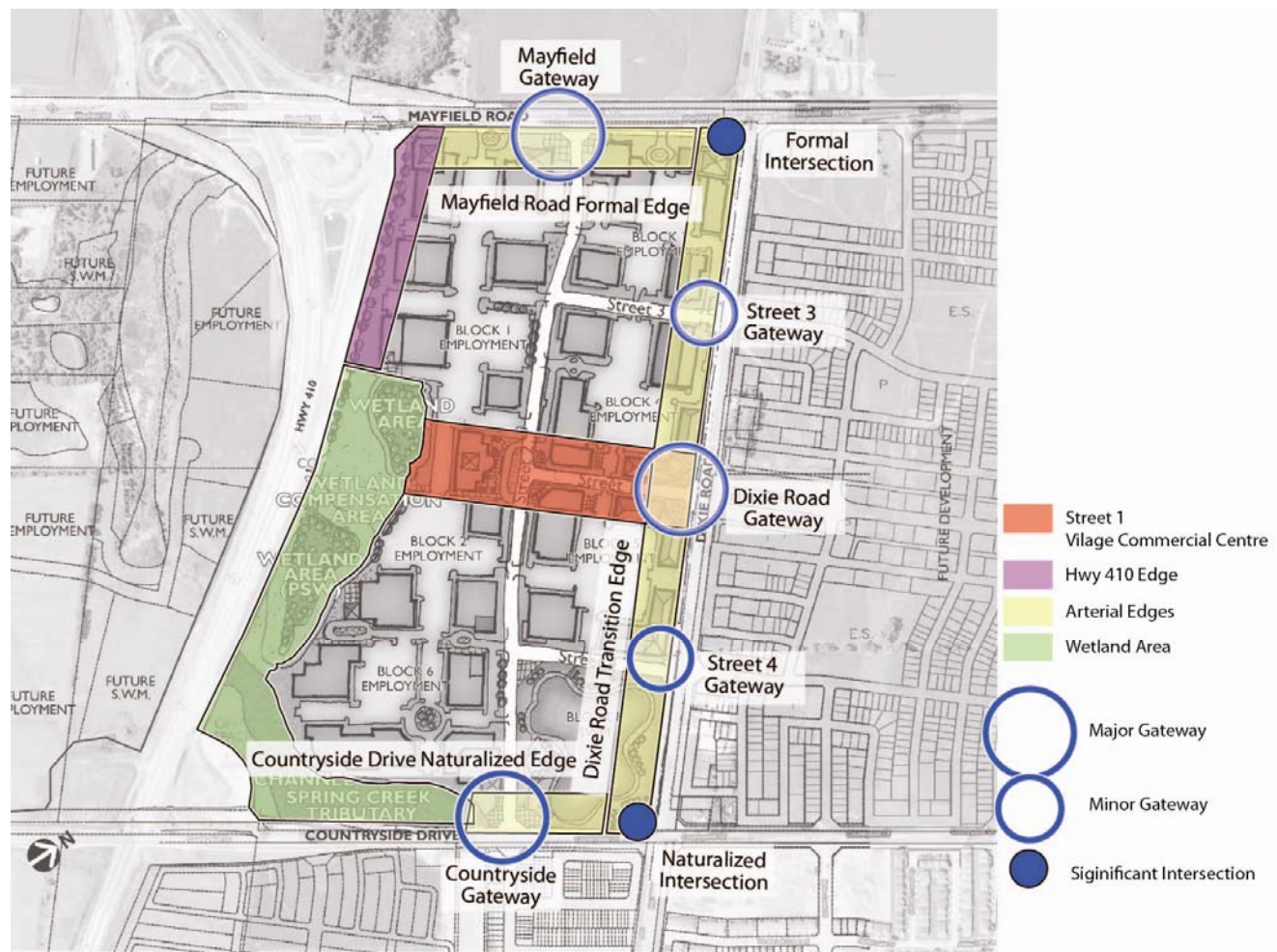
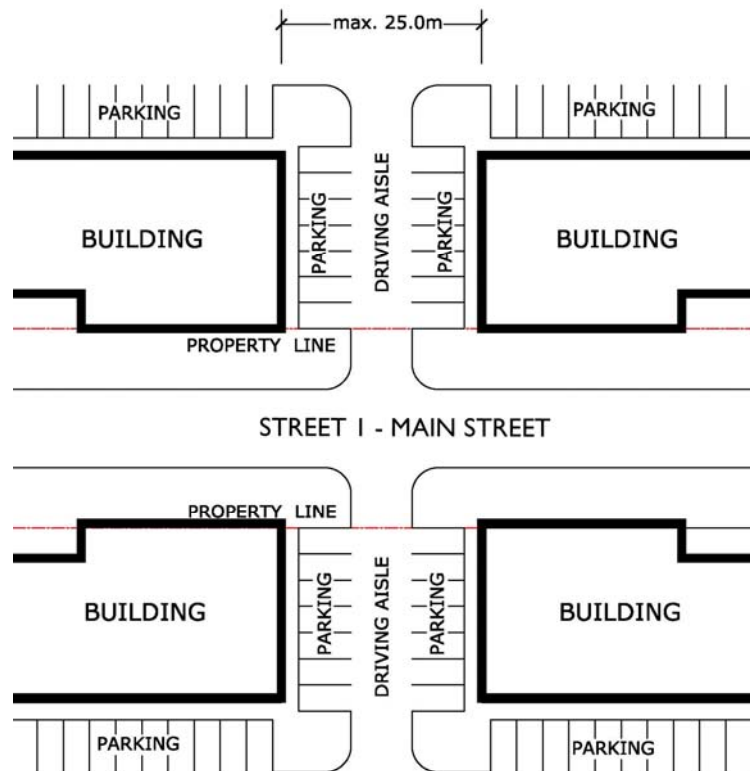


Figure 5.0 Special Character Areas

2.2.1 Street I - Village Commercial Centre (Terminus at Business Park Intersection)

Main Street (Street I) is the linking element between the employment and residential neighbourhoods of the Countryside Villages Community. The character and scale of this street, as established east of Dixie Road should continue to the west as well. The role and character of this special area will be reinforced through the following guidelines:

- Uses within the employment area should include mixed use, small scale office/retail and services oriented to the needs of the employment district, i.e. convenience food, day care, cafes, small rest areas, convenience store, cleaner/shoe repair, etc. and commercial outlets of benefit to the entire community.
- Buildings facing Street I shall have commercial uses at grade such as cafes, a convenience store, and a local restaurant, with office space above.
- In the service/small retail zone, the street should be hard surfaced from back-of-curb to building face, allowing for soft landscaping by way of planters and trees in grates.
- Generous provision of themed street furnishings to accommodate transit/pedestrian needs, i.e. bollards, newspaper boxes, transit shelters, etc. a private patio should be considered as well.
- The conceptual enhanced pedestrian crossing treatment and boulevard pavement enhancements will be determined at subdivision review and site plan stages respectively.
- Buildings along Main Street should frame the focal point feature at the Business Park Intersection.
- At the intersection of Street I and Dixie Road a built form features, like a tower or a unique structure with prominent massing should be provided with high quality materials to ‘complete’ the main street character.
- To maximize building frontage parking is not permitted in the ‘front yard’ setback.
- Buildings facing Street I shall generally be between 40m – 60m in length. Subject to site plan design/layout.
- Optimum distance of 25.0m is permitted between two abutting buildings fronting Street I. The 25.0m distance shall be dedicated to two rows of parking, a driving aisle and pedestrian walkways. All other parking shall be situated at the rear of the buildings.



Optimum distance of 25.0m between two abutting buildings

BUILDING TERMINUS (Focal Point Alternative A):

Planning Guidelines:

- Provide a special building that is acceptable as a high quality focal point to be placed at the end of Street 1 (Main Street) framing the Business Park Intersection.
- Terminus Building may have vehicular driveway at front (facing the Business Park Intersection).

Landscape Guidelines:

- Provide high quality planting features in keeping with the City of Brampton Flower Initiatives at front of Terminus Building (facing the Business Park Intersection), provide variety of shrubs and tree canopy.

Architecture Guidelines:

- Terminus Building should provide a primary entrance facing Street 1, and a secondary entrance facing the Wetland Area.
- The built form should incorporate high quality design, prominent massing and architectural detailing to act as a landmark at this location.

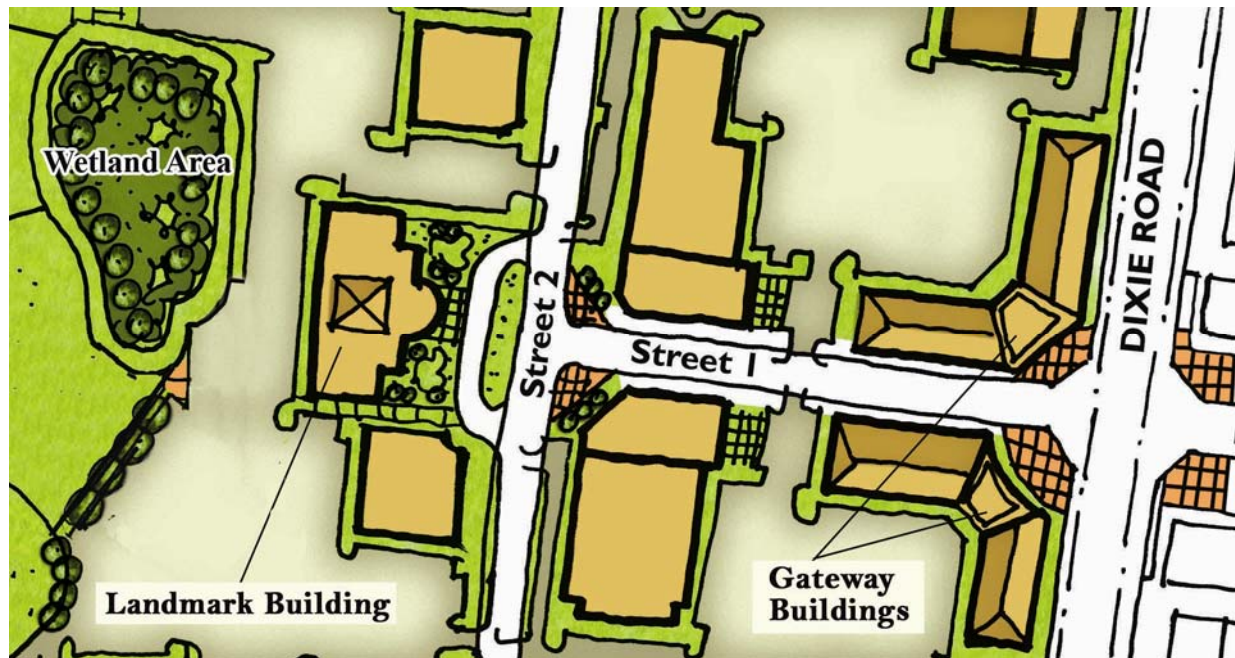


Figure 6.1 Neighbourhood Centre with Commercial at Grade, and a Building Terminus

WETLAND TERMINUS (Focal Point Alternative B):

Planning Guidelines:

- Provide a tree lines median at terminus of Street 1.
- Provide a focal point that terminates Street 1 (Main Street) in the form of a lookout or a parkette with hard landscaping features such as a low wall and lush planting.

Landscape Guidelines:

- Visitors parking may be provided as an on-street-parking zone with adequate soft landscaping such as shrubs and high tree canopy to balance paved and landscaped surface area.

Architecture Guidelines:

- Buildings framing the terminating street should have high quality architectural detailing and direct views onto the terminus street.
- The main entrance of the buildings shall be located close to the intersection of Street 1 and Street 2.
- Building façades should line up with front lot lines of the buildings along Street 1.

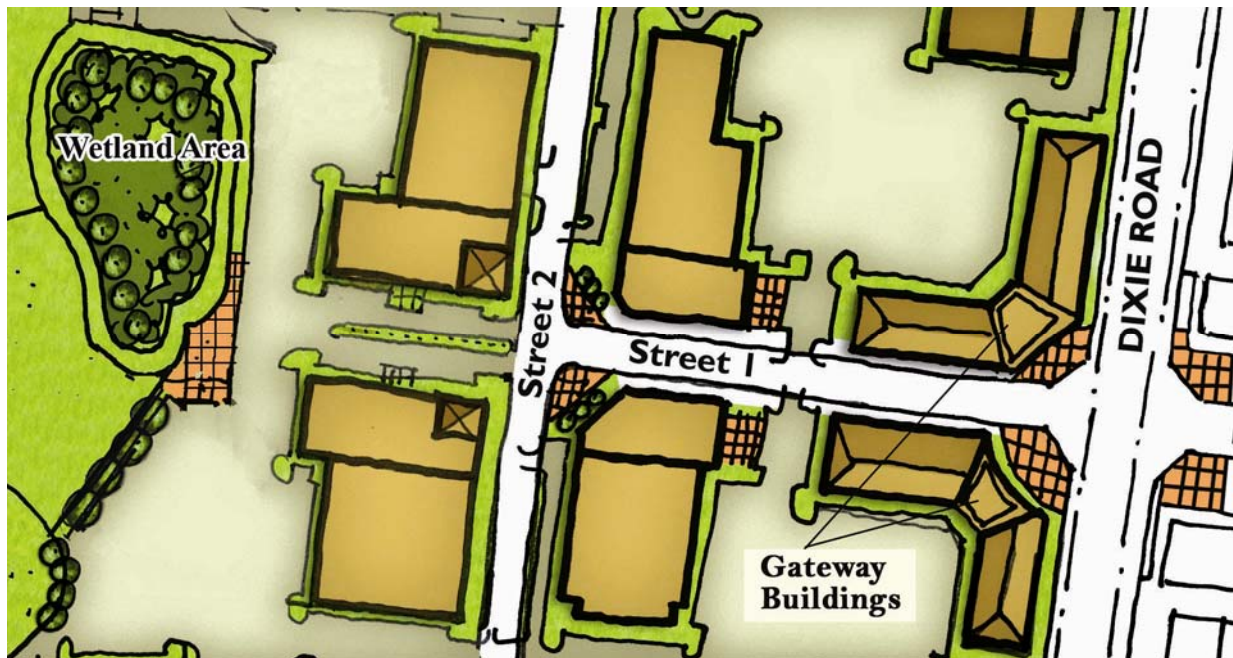


Figure 6.2 Neighbourhood Centre with Commercial at Grade, and Planted Median Terminus

PLAZA TERMINUS (Focal Point Alternative C):**Planning Guidelines:**

- Provide a 3m wide (minimum) pedestrian pathway privately maintained and leading toward the Wetland Area Lookout, with terminating view of the Wetland Area trees.
- Screen pathway from adjacent parking areas while maintaining views of the pathway for safety purposes.

Landscape Guidelines:

- Pedestrian pathway shall be landscaped with decorative pavement and interpretive signage.

Architecture Guidelines

- Buildings framing the pedestrian pathway should have high quality architectural detailing and direct views onto the pathway.
- Corner of buildings facing pathway shall incorporate a greater setback to create an open space leading to the pedestrian pathway.

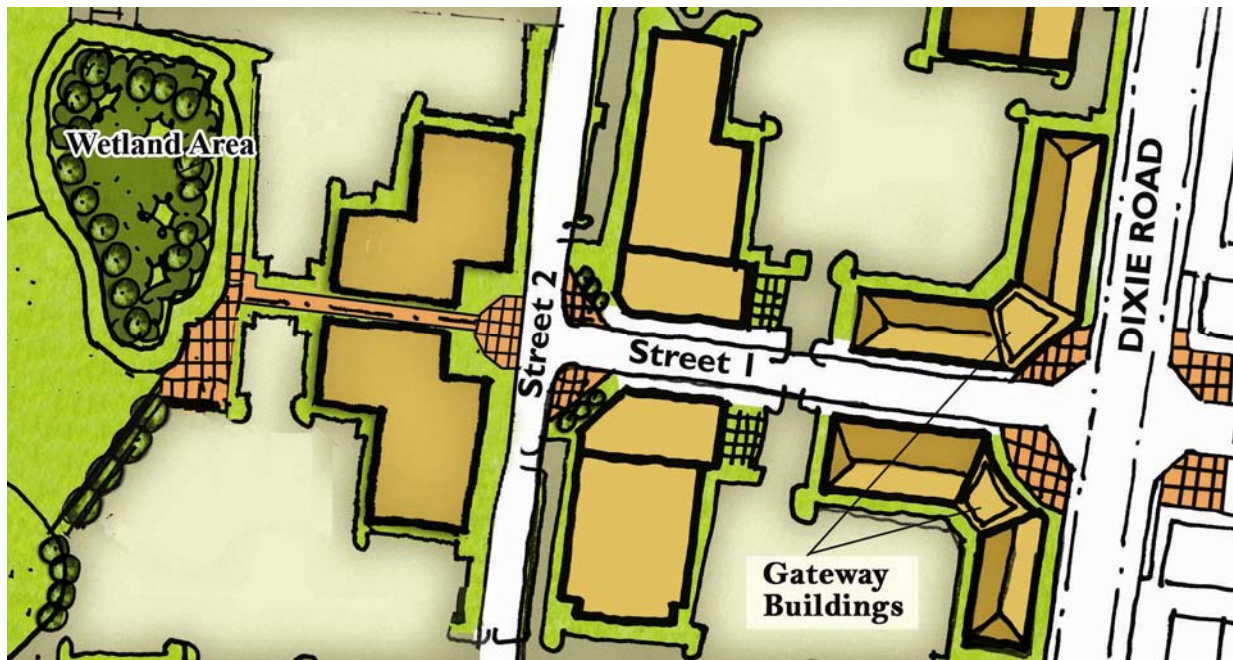


Figure 6.3 Neighbourhood Centre with Commercial at Grade, and a Pedestrian Pathway at Terminus

2.2.2 Hwy. 410 Edge

The area's highest quality, multi-storey industrial and office buildings with distinctive architectural elevations should be oriented toward the Hwy 410 and the off ramp to Mayfield Road. In keeping with the following guidelines, well designed and appropriately sited buildings shall take advantage of this more visually prominent location and where existing grade and existing vegetation permit be visible from the highway:

Site Planning Guidelines:

- Hwy 410 exposure should have first priority for the location of prominent/upscale buildings, where visibility permits.
- The siting of buildings and built form shall take advantage of the natural features and terrain.
- Buildings should be sited as close as possible to the highway to promote visibility of prestige office buildings from the Hwy 410. (Alternatively, a building may be set back further from the 410 edge in response to other site plan considerations, see Figures 7.0 and 7.1).
- Building frontages should be oriented to views from Hwy 410 corridor and Mayfield Road overpass.

Landscape Guidelines:

- Provide a minimum of 14.0m wide landscape buffer consistent with the MTO prescribed setback.
- Provide high quality formal landscape treatment in the form of tree canopy and low shrubs scaled to highway speeds and designed to be sufficiently transparent to allow views to upscale building facades.
- Provide berming and landscaping where a parking area abuts the landscape buffer.
- Provide signage scaled to highway speeds and if surface mounted well integrated with grading and landscaping.
- Focus site lighting away from highway. Provide night architectural feature lighting to Hwy 410 frontage as appropriate.
- Landscape architectural; features such as low walls, decorative metal fencing, planters, etc. should be directed toward traffic exiting the highway onto Mayfield Road and along the Mayfield Road frontage.

Built Form Guidelines:

- Provide a building type with high quality multi-storey architecture fronting Hwy 410 with light industrial uses at back.
- Higher quality architectural materials and details scaled for visibility at highway speeds should be used.



Architecture with a high window-to-wall ratio

2.2.2.1 Auto Mall Special Policy Area

While these CDGs have been prepared, the landowner has actively pursued tenants and appropriate uses for the Employment Lands East of Hwy 410. The landowner has recently secured a significant tenant to develop an Auto Mall for the northwest quadrant of the Employment Lands (Block 1).

Although detailed site plans have not yet been developed, the design of this quadrant (Block 1) will conform to the principles, objectives and guidelines set out in this document while reflecting the unique site plan and operational considerations of an Auto Mall use. The proposed Auto Mall will be located along the Hwy 410 edge, south of Mayfield Road, north of Wetland Area B and west of Street 2. This Auto Mall will benefit from significant visibility from Hwy 410 and represent the upscale character of the district.

The following set of guidelines shall apply to the Auto Mall development in Block 1, and supersede the guidelines in section 2.2.2 should the Auto Mall block is implemented.

- Minimum two storey building massing with ceiling-to-floor fenestration and high quality architectural materials shall be situated to face the highway. Transparency of activities inside the buildings coupled with higher quality architectural detailing shall distinguish it as a unique and upscale district.
- Vary building setback from the 14m (MTO/Hwy 410) landscape buffer should be considered to create a diverse edge and streetscape treatment;
 - Minimize setback from landscape buffer with auto display area and minimize landscape screening from Hwy 410. Incorporate dominant built form to address highway edge.
 - Alternatively, increased setback from Hwy 410 and minimum setback from internal road with significant landscape screening of parking area from Hwy 410. Two-storey building massing will be enhanced.
- The Mayfield Automall Gateway shall be incorporated at the intersection of Mayfield Road and Street 2 (section 2.2.3).
- Corner buildings shall incorporate high quality architectural and landscape treatment to celebrate the Auto Mall entrance.
- Site plans for internal corner lots shall ensure that the building or primary car display areas address the corner.
- Where buildings are set back to provide auto display area, the site plan shall incorporate signage and landscaping to celebrate the Auto Mall entrance.



Highway 410 Edge Architecture

Figures 7.0 & 7.1 illustrate two possible concept plans and demonstrate the additional site plan specific guidelines to those listed in section 2.2.2.1. The Motor Vehicle Commercial Development is subject to the *City of Brampton Automotive Services Guidelines, February 2010*.

Provide high quality architectural treatment where buildings are set close to the Hwy 410 edge off ramp and Mayfield Road intersection.



Figure 7.0 Conceptual Illustration of Auto Mall Development, Alternative A

Provide an identifying feature where building is set away from intersection.

In order to provide enhanced internal road streetscapes and effectively develop the block, set buildings at focal view of internal road.

Provide auto related signage (such as dealership logos) visible from Hwy 410.



Figure 7.1 Conceptual Illustration of Auto Mall Development, Alternative B

2.2.3 Arterial Road Edges

Buildings along Mayfield Road, Dixie Road and Countryside Drive should be industrial and office uses and should respect the following policies:

Landscape:

- Provide a minimum 4.5m landscape buffer at the streets.
- Only a single row of parking and drive aisle may be provided between the building and the arterial road.

Built Form:

- Provide a minimum building setback of 4.5m.
- Provide a maximum building setback of 20.0m.
- Minimize parking in front of buildings

Mayfield Road FORMAL EDGE:

- Buildings with prominent massing and well articulated elevations should be located parallel to the road to form a desirable street edge.
- Buildings should be designed with attractive rooflines with variations in building elements, materials, colours or massing.
- Street trees and buffer tree plantings should be provided along the edge to create an urban street character, complimentary to the sidewalk on Mayfield Road.
- Parking should be located internally.
- Adequate planting should be provided to screen parking wherever visible from Mayfield Road.



Landscaped arterial road edge (Formal Edge)



Stormwater Management Pond Edge along Countryside Drive (Naturalized Edge)

Countryside Drive NATURALIZED EDGE:

The Countryside Drive edge should incorporate an informal arrangement of soft landscaping dominated by trees. Buildings should face onto Countryside Drive with a sufficient setback edge. Complimentary to the Stormwater Management Pond, the road streetscape edge should continue an informal arrangement of soft landscaping consisting of a strong and attractive tree dominant landscape incorporating a variety of deciduous, coniferous and flowering trees. As well, a lookout/seating area located at a gateway or main transit stop will be considered. The naturalized edges abutting the SWMP shall be composed of organic arrangements of native trees, shrubs and perennials. The landscape buffer design on private lands will be determined during the site plan approval process.

Dixie Road TRANSITION EDGE:

This very important area is the interface between the planned residential community on the east side of Dixie Road and employment lands on the west side of Dixie Road. Transitional building design should be applied in the form of smaller office uses. The following design aspects should be incorporated:



Architectural Interface on Dixie Road Edge

- Locate buildings close to the street.
- Place buildings parallel to the street to form a strong street edge.
- Design buildings to provide an interesting streetscape. Commercial property edges shall have a refined landscape treatment including quality plant material or masonry elements and ornamental fencing.
- Buildings should be designed with architecture appropriate for an employment - residential interface
- Locate buildings entrances to face the street.
- Provide visitor parking in front of buildings and close to entrances.
- Provide safe and comfortable pedestrian connection to transit stops
- Buildings should have smaller building floor plates for the employment residential interface.



Dixie Road Edge Concept

2.2.4 Wetland Area

- The Wetlands and the Spring Creek Tributary (channel) shall be protected, enhanced and restored to facilitate habitat enhancement.
- Provide an environmental buffer around the Wetlands and the Channel to minimize negative environmental impacts associated with development.
- Plant material within the buffer area shall be native and drought tolerant.
- Buffer area should include native deciduous and coniferous species of various sizes and forms to support habitat and wildlife habitat (subject to review of the City and project Landscape Architect). Native vegetation is required as a continuum through these spaces.
- All buildings facing the Wetland Area should have high quality elevations while providing views onto the open space.



A restored wetland

2.2.5 Major and Minor Gateways

Gateways provide a special opportunity to create a sense of arrival and highlight the Countryside Village Employment Lands as a distinct place. These important locations present themselves as community focal points and therefore shall be addressed through high quality architecture and landscape design (see Figure 5 for Gateway Locations). Gateways should be similarly themed, while emphasizing some as Major Gateways by using unifying structuring elements and distinctive features.



Employment Land Formal Gateway

Major Gateways are identified at three entrance locations: Mayfield Road, Dixie Road and Countryside Drive. Minor Gateways are identified at two entrance locations, from Dixie Road, at Street 3 and 4. As well the intersection of Mayfield Road and Dixie Road is a designated Arterial Road Gateway. The following guidelines apply to all gateways:

- Built form and landscaping at gateway locations shall be coordinated and consistently designed to provide a harmonious visual entry into the employment lands.
- Architectural built form is encouraged to represent the gateway. It should be well designed with high quality materials and interesting memorable architectural elements.
- Architectural elements such as walls, columns, towers, pergolas, and landscape features such as planters, shrubs, flowers should be incorporated and coordinated to emphasize the gateway.
- Gateway intersections shall be coordinated with the City of Brampton's "Gateway Beautification Program" to reinforce Brampton's image as Flower City.
- The intersection at Mayfield Road and Dixie Road is a City Gateway and should incorporate the City of Brampton corporate identifiers and include a historic interpretive element addressing the Mayfield and Dixie road intersection and inn.
- Signage, decorative fences and landscaping should be incorporated into minor gateways to employment areas.
- Landscape features such as signage, plantings, and other hard landscape features should be oriented to face and address vehicular and pedestrian traffic entering and leaving the Employment Lands.

Key design and character considerations specific to each gateway:

1. Mayfield Road AUTO MALL GATEWAY:

Mayfield Road – is the main access route into the employment zone Auto Mall development. The gateway is framed by the Auto Mall block to the west, and the Motor Vehicle Commercial block to the east.

West of Street 2, intersection treatment to either the corner building or the significant landscape element as described in Figure 7.0 and 7.1 (subject to Site Plan approval).

Site planning, landscape and architectural treatment of the Motor Vehicle Commercial block (east of Street 2) is subject to the *Automotive Service Centres Guidelines*, February 2007 (City of Brampton). Appendix A.3 of the *Automotive Service Centre Guidelines* illustrates gateway appropriate treatment where the principle building is set to face the corner.

For entry blocks into the employment zone (both west and east of Street 1), the streetscape should be of ‘formal character’ representing the upscale nature of the employment lands and the Auto Mall, and enjoy high quality site maintenance. To accomplish this, the use of interesting built form, quality materials, and landscaping should be implemented.

2. Countryside Drive OPEN SPACE GATEWAY:

Countryside Drive – is a gateway characterized by open space, a natural heritage corridor to the west and SWM pond on the east. The gateway character on both sides of Street 2 should be composed principally of informal soft landscaping with particular attention to ‘Flower City’ initiatives. A look-out/seating area to the SWM pond will be considered if permitted by the SWM pond engineering requirements.

Similar to the Dixie Road edge buildings adjacent to Countryside Drive may provide direct pedestrian access from the street. Parking areas located between the building and the street should be avoided. High quality architectural materials and detailing shall be provided to create an inviting entrance to the community.



Countryside Drive Gateway Concept

**3. Dixie Road at Main Street
NEIGHBOURHOOD GATEWAY:**

Dixie Road – is the gateway where Main Street enters the Employment Lands, connecting the neighbouring Countryside Villages residential community. The abutting buildings should be located at the street line, and provide pedestrian related access and pedestrian amenities, such as outdoor cafés, seating space, displays, awnings, etc. Landscaping along Dixie Road should incorporate an urban theme with hard and soft elements, representing a visually diverse edge.



Transitional Offices at the corner of Dixie Road and Street I

To provide a visual and a functional transition from the larger industrial/commercial enterprises, this key gateway should have a more residential scale massing and design details.

Site planning should face the majority of buildings to Dixie Road and provide direct pedestrian access from the street. Parking areas should be provided at the rear or to the side of buildings. Small visitor parking areas with low height visual screenings across their frontage may interface with Dixie Road.

4. Dixie Road at Street 4 MINOR GATEWAY:

This is currently a non-participatory property (hold-out property) that should include free standing/ service uses as options. Figure 5.0 demonstrates the preferred incorporation of this site. The provided illustration to the right demonstrates a potential concept if the non-participating property is not developed in unison with the adjacent properties.



Alternative Concept Sketch

2.2.6 Significant Intersections

Mayfield Road and Dixie Road FORMAL INTERSECTION:

This intersection can contribute to the landmark or gateway character of the Employment Lands with the provision of well designed and sited buildings and inclusion of other elements.

- Enhance significant intersections design by providing interesting architectural built form massing and enhanced landscaping. This should be achieved by situating buildings with at least 2 storey massing close to the street to enhance business prominence and accentuate the corner.
- Design buildings so that both façades facing the arterial streets are well articulated and sufficiently fenestrated.
- 60- 70% of building façade at corners shall be glass.
- Provide interesting building elements at the corner, such as accent finishes and roof detailing.
- Place main building entrances at the corner to be clearly seen from the adjacent road.
- Defined intersection through hard and soft landscape features such as decorative pavement, and flowering plants in keeping the ‘Flower City’ Initiatives.
- Provide a pedestrian amenity space at the corner, for example, in the form of a small piazza incorporating an interpretive element for Mayfield and Dixie Roads Intersection and Inn.
- As Mayfield Road is also a City of Brampton edge, corporate identifiers shall be incorporated into the gateway design.



Intersection Architecture

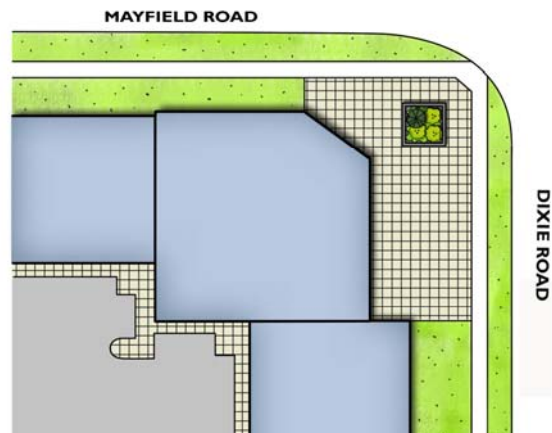


Illustration of Mayfield Road Intersection

Countryside Drive and Dixie Road
NATURALIZED INTERSECTION:

This intersection provides views of the Stormwater Management Pond. As such, the stormwater management pond should be treated as a gateway feature into the community and provide appropriate landscaping.

Include a landscape element with a small pedestrian amenity space at the corners for SWM pond viewing opportunities. Stormwater Pond Intersection design should respond to the new City SWM pond design guidelines.



SWM Pond Concept Sketch with Naturalized Intersections



SWM Pond Significant Intersection Soft and Hard Landscaping Treatment

3.0 SITE PLANNING AND BUILT FORM PRINCIPLES

Commercial building elevations in the interior of the Employment Lands should apply common elements such as continuity of building materials, colours and detailing. This design approach will allow for a visually coherent development. The common architectural elements that should be provided on all of the buildings include: the use of architectural features that will place an emphasis on building entrances (3-dimensional relief), use of projecting canopies or arcades to promote pedestrian comfort, and high quality low maintenance materials. All buildings are required to be oriented towards the streetscape to develop street side character through proximity relationships. This also provides opportunities for streetscape detail design that will visually connect the neighbourhood. Buildings which are placed within high visibility of Hwy 410 should be designed to enhance views and express the upscale character of the Employment Lands. Loading and servicing areas along public streets should be avoided. The following sections describe the detail design guidelines.

3.1 Site Planning and Design:

- Locate high quality, multi-storey office buildings along the Hwy 410, larger office/industrial uses in the interior of the block and smaller residential scaled buildings along Dixie Road.
- Create interesting views from the adjacent roads with unique building design that take advantage of public exposure.
- Orient buildings parallel to the street to form a strong street edge.
- Support pedestrian scale and character through appropriately massed streetscape elements, detailing, consideration of microclimates, and provision of adequate pedestrian linkages.
- Place significant buildings at corners and street intersections.
- Define the arterial intersections and street corners as major gateways.
- Minimize the impact of parking areas through the use of landscaping features and efficient parking configuration.
- Locate buildings in close proximity of public transit and walking routes.
- Each subsequent development phase should be compatible and coordinated in architectural character and landscape character, site planning, and design with previous phases.



Example Highway Architecture

3.2 Built Form and Massing
(also refer to Section 2.2.2)

- A variety of architectural styles, building massing and articulation is encouraged. The site layout, architectural style, massing, detailing, colours and materials of each building should be complementary with the surrounding buildings to maintain a level of overall harmony in the streetscape.
- Where a larger building is planned for a site, it should be integrated through site layout and detailing to complement the surrounding buildings.
- Built form and massing should promote an interesting, human scale environment through depth and height variations for visually stimulating architecture.
- To establish appropriate scale, the roofs of large building types are encouraged to have visual accenting of parapet lines, or other elements to provide a similar visual enhancement.
- Roofs shall incorporate screening of the mechanical units from all public views.
- Place upscale buildings on prominent locations. Where these locations are on a corner lot, locate the main entrance of those buildings at the corner to enhance relationship with the streetscape and intersection.
- Maximize the amount of built frontage on arterial and collector streets.
- The building site layout and massing, architectural elements, colour, materials should be compatible with each other and provide an overall common theme.

Auto Mall Block

- Minimum two-storey building massing with ceiling-to-floor fenestration and high quality architectural materials shall be situated to face the highway.
- Transparency of activities inside the buildings coupled with higher quality architectural detailing shall distinguish it as a unique and upscale district.



Broken form to create interest



Canopy over building entrance



Pedestrian Oriented building entrance

3.3 Building Elevations

- Elevations should be well articulated and provide visual interest by changes in plane and parapet height, and designed to avoid long monotonous facades.
- Elevations should be pedestrian friendly through application of appropriate scale, detailing, and architectural elements such as entry or weather protective elements, fenestration, etc.
- On corner lots the same attention should be given to both elevations fronting on the streets.
- Canopies, arcades and other façade elements should be provided in pedestrian areas, to enliven the street frontage, promote visual variety and promote pedestrian comfort.
- Building materials include glass, stucco, steel and precast to create inspiring building facades. Other materials will be reviewed on their own design merit.

3.4 Building Entrances

- Locate building entrances facing the streets wherever possible.
- Entrances should be designed to be recognizable with the use of distinctive architectural entry elements such as pergola, awning, etc., and provide protection from the weather.
- Visitors parking to be easy accessible to the main entrance.

3.5 Pedestrian Circulation

- Promote a continuous pedestrian network (sidewalks and pathways) to support safe and comfortable pedestrian movement at grade, between sites and within them.
- Provision for barrier free environment should be integrated where necessary.
- Pedestrian connections to future transit stops should be safe, convenient, legible and well signed to encourage transit use.
- Provide direct pedestrian access to parking areas and link to pedestrian network.
- Pedestrian and vehicular crossings should be minimized and distinctly marked using soft and hard landscaping features to promote pedestrian safety and easy of orientation.
- Provide pedestrian links to adjacent collector roads and adjacent community.
- Provide bicycle storage racks close to the building entrances.



Defined pedestrian and vehicular routes

3.6 Vehicular Access, Parking and Servicing

- Vehicular access to development sites should be provided from local and collector streets wherever possible.
- Provide clear vehicular access routes through the site.

- Distinguish primary vehicle routes through the site with the use of signage, curbing, paving and planting.
- Design buildings with shared driveway access.
- Design service areas in such a way so not to conflict with parking.
- Visitor parking to be easily accessible to the main entrance.
- Major parking areas should not be located between the building and the street (this guideline does not apply to the Auto Mall Block, see section 2.2.2).
- Locate parking behind or at the side of the building and use landscape to screen views to parking.
- Locate parking areas in proximity to the office component of the building and within view of the building entrance.
- The separation distance between site access driveways and street intersection should be maximized.
- The scale of large parking areas should be reduced by introducing parking islands and other measures.



Vehicular Access to Building



Shared driveway access between buildings

3.7 Loading, Servicing, Utilities and Outside Storage

- Loading and servicing should be oriented away from prominent views. If the building siting does not permit proper orientation significant buffering, berming, landscaping, and/or architectural elements shall be required.
- Locate servicing and loading areas away from the streets and public exposure.
- Mechanical units for all buildings should be integrated within the architecture of the building or appropriately screened.
- Utility pipes and meters shall be located away from prominent facades.
- Utilities should run internally within the buildings.
- *Outside storage shall be located at the rear of the building the principal building, not visible from the street
- Storage area shall be screened with appropriate landscaping and shall not store hazardous goods
- Outside storage shall be screened by fence, planting or equivalent visual barrier
- Storage area shall not encroach onto buffer area

*** Auto mall outside car display area is not deemed to be ‘outside storage’ and is excluded from these guidelines. Auto mall outside car display areas will be dealt with at the site plan approval stage.**

4.0 LANDSCAPE GUIDELINES

4.1 Goals, Objectives and Principles

- Articulate the Stormwater Management Pond and the Wetland Area as key landscape features for the employment lands.
- Landscape Plans should be developed with consideration of future residential development to the east of Dixie Road that will provide visual relief from the developed elements as well as passive recreational opportunities.
- Focus on visual enhancement of the public realm.
- Achieve compatibility with adjacent future residential landscaping.

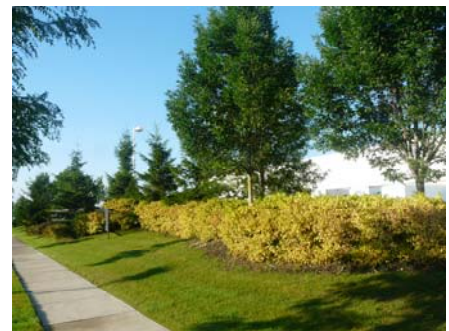
4.2 Landscape Zones

4.2.1 Landscape Buffers and Screening

Landscape buffers and screening shall be incorporated to effectively buffer the adjacent parking and undesirable views along arterial roads, collector roads and Hwy 410.

- Ensure that the landscape buffer contains coniferous plant materials that are visually effective in all seasons.
- Arterial and Hwy 410 buffer plantings shall consist of a mix of coniferous, deciduous and flowering trees.
- Consider a mix of deciduous and coniferous trees/shrubs to provide year-round interest.

Setback buffers were established to protect both of the wetlands from negative impacts associated with development. In addition, the EIR/FSR will finalize the size and extent of the compensation area for removal of an existing wetland in block 2. Setback buffers for these wetlands and the Spring Creek Tributary channel have been established to protect, restore and enhance the environmental functions of the natural heritage features. The buffers shall be stabilized and naturalized to protect and enhance the native aquatic habitat and water quality. Subdivision landscape plans for these areas will be subject to the standards, review and approval of TRCA and the City.



Buffer Planting Examples



Example of Pedestrian Connections to Building Entrances

4.2.2 Pedestrian Circulation

In order to create a comfortable, safe and efficient pedestrian network the following guidelines should be addressed:

- Landscape islands in parking lots should be provided where pedestrian connections and linkage occur, to facilitate pedestrian circulation.
- Street furniture for pedestrians, such as seating, garbage receptacles and bicycle storage racks should be provided.



Example of Pedestrian Connection within parking lots

4.2.3 Corners

- May be appropriate locations for bus/transit stops.
- Landscaping shall coordinate with corner built form.
- Significant intersections on Dixie Road will also provide gateway elements to reinforce a sense of arrival into the Employment Lands and Residential Community of Countryside Villages.



Landscaping to Accentuate Corners

4.2.4 Gateways

The Major gateways are located on the corner of Mayfield Road and Street 2, Dixie Road and Street 1, and Countryside Drive and Street 2. See Figure 2.1 Community Structure.

- Built form and landscaping in gateway areas shall be coordinated and consistently designed to provide a harmonious entry to the Employment Lands.
- Signage shall be highly visible.
- Masonry features at entry ways shall be incorporated in gateway designs.
- Flower City Strategy shall be implemented.

4.2.5 Stormwater Management Pond

The stormwater management pond will be a key landscape green space feature of the Employment Lands.

- It should be publicly accessible.
- Will include a seating area(s) where look-out opportunities exist in combination with gateways and/or transit stops.
- Should be highly visible from adjacent streets.



Stormwater Pond

- Landscape planting is required along the perimeter and throughout the pond block area and will be subject to pending City SWM pond guidelines.
- Will use native plant materials characterized by trees, shrubs and vegetation that promote a naturalized cover and habitat to define the pond edge; similar to the adjacent natural heritage system (Channel).
- Wetland plant species shall secure the banks of the pond.
- Where ponds abut a road, create an urban edge treatment by planting a continuous row of street trees along the street edge.
- Plant materials shall distinguish between manicured formal landscaping areas and the naturalized edge of the stormwater management pond facility.



Transition from Informal Landscaping to Formal Landscaping along the SWM Pond Edge

4.2.6 Channel Blocks and Open Space

The existing state of the channel and open space blocks should be maintained and enhanced with native plantings in accordance with TRCA guidelines. The impact of development close to natural heritage areas shall be minimized during site development and construction to negate any negative impact on the existing natural environment. Restoration and compensation landscape plans will be reviewed and approved by the City and TRCA.

4.2.7 Parking

- Large parking areas should be divided with use of landscaping including shrubs and trees.
- Landscaping should be provided to screen parking from streets.
- Parking areas shall be paved with hard surface material.
- Parking islands should be considered to include flower beds.

4.3 Landscaping Elements

4.3.1 Street Furniture

Street furniture within the employment lands can include landscape elements such as:

- Lighting, (City standards),
- Benches (City standards),
- Trash receptacles (City standards),
- Bike racks (by site plan agreement) and
- Bus shelters (City standards).

These elements should reflect the character of the proposed building elevations, be located in prominent locations relevant to pedestrian circulation and should be constructed of high quality materials. Bus shelters should be provided in safe and visible locations along transit routes.



Street Furniture

4.3.2 Lighting

- Enhance the night image by highlighting prominent architectural and/or landscape elements, corporate identity features, and displays (accent lighting may be used to emphasize built form and landscaping).
- Select fixture type, size and locate lighting to enable users to easily identify pedestrian and vehicle routes through the site.
- Ensure illumination levels in parking areas provide for safe use.
- Street lights shall be located to light any open space vista areas and outdoor pedestrian courtyards.
- Strengthen a safe pedestrian environment by providing appropriate lighting in all pedestrian areas including building entrances, walkways, and parking areas.
- Ensure lighting is compatible with the characteristics of the furniture and signage within the site.
- Lighting should be directed downward by means of lighting fixtures or orientation of lighting to reduce light pollution and light spill shall not encroach into natural heritage features or their buffers.



Well lit pedestrian walkway

4.3.3 Fencing Design Guidelines

Fencing details will be subject to review and approval at the site planning stage. The following principles should be considered.

- Decorative metal fencing with masonry columns to be placed at approaching gateways and strategically along roads, subject to site planning review and land uses.
- Chain link fencing shall be located along the stormwater pond edges and open space for permeable views from adjacent employment lands and roadways in accordance with City standards.



Masonry Columns and Decorative metal fencing example



Chain Link fencing to allow permeable views to open

4.3.4 Signage

- Ensure that signs are clearly visible from public streets, and include the name of the building or tenant. Individual cut out letter and / or logos are preferred.
- Provide a building numeral/address sign that is clearly visible for traffic along roads where property is addressed.
- Integrate corporate identity into the materials, colours and details of the elevation design and hard and soft landscape elements.
- Hard and soft landscaping elements should be incorporated into the design of the signage.



Corporate Signage

4.3.5 Street Trees

Street trees are aesthetically pleasing landscape elements that enhance the streetscape and define the street edge. The following street tree design guidelines include:

- Street trees shall be provided along the entire road system to define the street edge and co-ordinate with streetscape elements such as lighting, signage, fencing etc.
- Street trees will be provided on both sides of the road within the road right of way boulevard, per City standards.
- Street trees shall be planted consistently along the street at 12-18m on-centre, or closer depending on species selection;
- Diversity, hardiness and aesthetic value shall define the species selection.
- City standards with regard to species selection and planting location shall prevail.

5.0 IMPLEMENTATION

5.1 Introduction

The Community Design Guidelines are submitted at the Block Plan stage as a vital part of the development approval process. It will provide a base for detailed landscape drawings and design review process during the subdivision and Site Plan stage approval process for the Employment Lands (East) -48-1.

5.2 Outstanding Work

It is to be understood that some outstanding work may proceed after the Community Design Guidelines have been submitted and may result in further changes to the information contained in this document regarding the Design Guidelines and Design Plan. Please refer to Section 1.2.

5.3 Conformity to the Community Design Guidelines (CDG)

If it is determined, in the opinion of the City, that a site plan deviates from these approved Community Design Guidelines, then a Design Brief will be provided by the applicant for approval by the City. Prior to the first submission of the Design Brief, the Control Architect recommends that the document be reviewed by them for conformity with the intent of the Community Design Guidelines, prior to submission to the City. In the case of site plan submissions, the Control Architect recommends that the plans be reviewed for conformity with the intent of the Community Design Guidelines, prior to submission to the City. In both instances the Control Architect comments will be coordinated with the City.

This privately administered Design Review process coordinates the site planning, architecture and landscape design of the proposed development.

5.4 Cost Sharing

The following table, Cost Responsibility Matrix, identifies cost responsibilities for proposed upgraded finishes, treatments and furnishings for site development elements of municipal and private ownership. The City's DC (Development Charges) bylaw reimburses proponents for the design and construction of municipal works included in the DC bylaw and subdivision agreement. Cost reimbursement is based on current City minimum development standards.

Works proposed by this Community Design Guideline and agreed to by the proponents beyond the minimum City standard are the proponents' cost responsibility. The following matrix summarizes these cost responsibilities.

Capital Cost Responsibility

| | Capital Cost City Responsibility (DC funded) | Capital Cost Developer Responsibility (Developer funded) |
|---|--|--|
| 14m Buffer from Hwy 410 | | |
| Planting to MTO Standards | | |
| Entry Elements/Features (Gateways) | | |
| Decorative masonry elements and signage, planting, water service and paving | | |
| Wetland Area | | |
| Topsoil, seeding, planting restoration of areas disturbed by construction | | |
| Planting within 10.0m landscaped buffer | | |
| Signage | | |
| Maintenance road | | |
| Wetland Compensation Area | | |
| Topsoil, seeding and planting | | |
| Planting within 10.0m landscaped buffer | | |
| Top of bank plantings | | |
| Channel & Naturalized Areas | | |
| Planting within 10.0m landscaped buffer | | |
| Planting within naturalized areas | | |
| Stormwater Management Facilities | | |
| Topsoil, seeding, sodding, aquatic woody shrub and tree planting | | |
| Signage | | |
| Maintenance road | | |
| Lookouts/view points/: paving retaining walls, barriers, benches and waste bins | | |
| Streetscape | | |
| Street lighting | | |
| Street Trees | | |
| 70mm cal deciduous street trees, any upgrades to size or density; topsoil and sod within regional and municipal road Right-of-ways. | | |

5.5 Conclusions

The design and approval process is iterative, changing in response to new information and proponent objectives and standards plus process at the City. However, the intent and objectives of these Community Design Guidelines remain a critical element of the approval process. The Control Architect/Landscape Architect authors of this CDG, recommend their continued involvement in the review of required Design Briefs and /or site plan submissions prior to formal submission to the City of Brampton. The Control Architect will review the documents and provide timely comments to the proponents, consultants and the City.

6.0 GREEN ECONOMIC PRINCIPLES

Following recent global trends and to promote the future initiatives of the City of Brampton’s “Green Economic Development” strategy, the encouragement of environmentally sustainable green industry within the employment lands will help attract business types and maintain Brampton’s competitive position as a location for business investment. The following principles are encouraged:

- Green Building and Energy Efficient Technology,
- Renewable Energy,
- Transportation and Supply Chain,
- Creation of Green-Collar Jobs,
- Green Financial Markets,
- Sustainable Business Practices,
- Programs/Incentives